



Local Government Division
Local Government Engineering Department
Municipal Governance and Services Project (MGSP)
IDA Credit No: 5339-BD

Report on Social Management Plan
Package No: MGSP/RAN/2017- 2018/ W-04

Social Management Plan

ELLENGA POURASHAVA

Name of Subproject:

a) Construction of 33 m RCC Bridge at Pathalkandi Channel under EllengaPourashava, Kalihati, Tangail

b) Construction of approach road by bituminous carpeting for 33 m RCC Bridge at Pathalkandi Channel (Ch.0+000 to 0+103 m, 0+136 to 0+ 970 m and link 160 m)under EllengaPourashava, Kalihati, Tangail.



Design, Supervision, and Management (DSM) Consultancy Services

Joint Venture of Hifab International AB, Sweden and

AQUA Consultant & Associates Ltd., Bangladesh



SEPTEMBER -2017
ELLENGA POURASHAVA.
KALIHATI, TANGAIL

Table of Content

No	Contents	Page. No
1.	Executive Summary	1
1	Introduction	1
1.1	Project Background	1
1.2	Legal and Policy Framework	1
1.3	Brief Description of Subproject	2
1.4	Social Management Plan	4
2.	Description of Subproject Area	4
2.1	Brief Profile of Ellenga Pourashava	4
2.2	The Subproject Location and Area Profile	10
2.3	Historical and Cultural Heritage Sites	5
2.4	Beneficiary Communities and Affected Persons	10
2.5	Gender and Vulnerability	10
3.	Social Impact Assessment	11
3.1	Benefits of the Subproject	11
3.2	Social Screening and Safeguard Compliance Issue	11
4.	Consultation and Community Participation	11
4.1	Stakeholder Analysis	11
4.2	Consultation and Participation plan for subproject	12
4.3	Consultation Outcomes – Issues, Concerns and Recommendations	12
5.0	Social Management Plan for site selection and design	12
5.1	Subproject Selection Process”.	12
5.2	Subproject Design Process	13
5.3.	Grievance Redress Mechanism (GRM)	13
5.4	Implementation Arrangement	14
6	Conclusions on Social Management Plan (SMP)	14
Photograph/Figure		
1	Subproject Estimated cost	3-4
2	Photograph on Existing foot bamboo bridge condition	2
3	Photograph Peoples faced difficulties due to risky bridge	3
4	Photograph Community consultation	13
5	At glance of Ellenga Pourashava	8
6	Sub project layout plan (W-4) Ellenga Pourashava	9
ANNEXURE		
1	Social Screening Report	1-12

Abbreviations

ARP	Abbreviated Resettlement Plan
B MDF	Bangladesh Municipal Development Fund
LAP	Land Acquisition Plan
CBO	Community Based Organization
CC	Cement Concrete
RCC	Rangpur City Corporation
CIP	Capital Investment Plan
CUL	Compensation-Under-Law
DC	Deputy Commissioner
DALY	District Land Acquisition Committee
DUTP	Dhaka Urban Transport Project
GoB	Government of Bangladesh
GRC	Grievance Redress Committee
GRAM	Grievance Redress Mechanism
HCG	House Construction Grant
HTG	House Transfer Grant
IDA	International Development Association
IP	Indigenous People
IPP	Indigenous Peoples Plan
KII	Key Informants Interviews
FGD	Focus Group Discussion
LGD	Local Government Division
LGED	Local Government Engineering Department
M& S	Management & Supervision
MGSP	Municipal Governance and Services Project
MOGRDC	Ministry of Local Government, Rural Development & Cooperatives
MSP	Municipal Services Project
MSL	Mean sea Level
NGO	Non-governmental Organization
PAH	Project Affected Household
PAP	Project Affected Person
PMU	Project Management Unit
RCC	Reinforced Concrete Cement
RP	Resettlement Pan
SIA	Social Impact Assessment
SMP	Social Management Plan
SSS	Social Safeguards Specialist
TLCC	Town Level Coordination Committee
ULB	Urban Local Body
WB	World Bank
WC	Ward Committee
RAP	Resettlement Action Plan

EXECUTIVE SUMMARY

Ellenga Pourashava is C Category Pourashava having 23.19 sq. areas. The ULB is densely populated having 9 administrative wards which are also underserved in all development considerations. The subproject has been selected considering the needs of the locality which is priority as CIP of Ellenga Pourashava.

The Social management Plan (SMP) of the Subproject under Ellenga Pourashava was developed through inclusive participation of all level stakeholders and using participatory approach. The SMP includes one social screening. The subproject comprises interventions proposed as per CIP-1 of the Ellenga Pourashava.

The sub project will benefit two of the 9 administrative wards of the Ellenga Pourashava (Wards and 6). According to engineering design, implementation of this subproject will be carried out within the existing right-of-way hence no LA is involved and no RAP is required.

Total cost of the subproject is Taka: 37,727,156.25

Main Purposes of the subproject

To improve safe transport networks construction 33 m long bridge, 937 m approach road and 160 m link road located in two wards. Presently there is a bamboo foot bridge requiring change of bamboo twice a year. It is very difficult and risky to cross the canal by Bamboo Bridge.

Anticipated Impacts:

The subproject will benefit 4 & 6 of 9 wards of the Ellenga Pourashava. About, 10,000 people of the two wards will be benefited for construction the bridge and approach road.

There is no obstacle in this proposed subproject area and none of PAP will be affected. The subproject will need shifting of 4 electric poles & cutting of 30 trees.

The local stakeholders are supportive of the subproject hence it is recommended to include it under MGSP.

INTRODUCTION

1.1. Project Background

The Government of Bangladesh, through the Local Government Engineering Department (LGED) and Bangladesh Municipal Development Fund (BMDF) have been implementing the Municipal Governance and Services Project (MGSP). The project aims to improve municipal governance and basic urban services in participating ULBs. Two physical components of the project include (i) Municipal Governance and Basic Urban Services Improvement (Component 1) to provide financial support to 26 pre-identified urban local bodies (ULBs) comprising 22 municipalities and four city corporations, and demand-based sub-credits to eligible ULBs for basic urban services improvement investment costs (component 2). Total project cost is **BDT 2,470,93.92** to be financed at 80:20 IDA loan to GOB contribution. LGED is implementing the Component 1 while BMDF is implementing component 2 of the project.

LGED is supporting the 26 pre-selected ULBs for investments in development and rehabilitation of Road, Bridge, Box-culvert, pedestrians bridge, street light, Road Safety, Bus terminal, Truck terminal, Boat landing Jetty, Drain, Retaining wall, Kitchen market, Cattle market, slaughter house, Public toilet, Solid waste management, Sweeper colony, Park, Community Centre and Whole sale market. LGED is financing through (i) base allocation based on population category of ULBs; (ii) performance-based allocation and (iii) operations and maintenance support on a declining basis over the project period.

The LGED component interventions have largely been developed for construction and rehabilitation within existing available land owned by the ULBs and acquisition of land has been avoided in the screening process. Removal of some obstruction and relocation of some businesses were involved in case of some sub projects for unavoidable circumstances. The MGSP did not avoid taking-up of sub projects in areas inhabited by tribal peoples and instead ensured their inclusion and participation where applicable. The ULBs addressed the removal of obstructions and in most cases the land entirely belongs to the ULB. In some instances, land of other GOB agencies like the BWDB, R&H, Railway or other local government like adjoining UP is needed and in such cases the ULB ensured getting such land by mutual understanding and proper documentation of such understanding. Relocation of business and removal of obstructions were resolved by consultation and agreement with the concerned PAPs and these are properly documented. Thus land acquisition, population displacement and tribal people's issues have been addressed following the country's legal framework and the World Bank policy on social safeguards.

1.2 Legal and Policy Framework

Given the approach of subproject preparation and implementation, the World Bank's Operational Policy (OP) on Involuntary Resettlement (OP 4.12) and on Indigenous Peoples (OP 4.10) triggered to the project. A Social Management Framework (SMF) has been adopted by LGED for the project that meets the requirements of the country's legal frameworks in Bangladesh "The Acquisition and Requisition of Immovable Property, Ordinance, 1982" and the Bank requirements including OP 4.12 and OP 4.10. The SMF also requires that subprojects are prepared ensuring inclusion, participation, transparency, and social accountability. Subprojects are prepared by respective ULBs in a process complying with the SMF requirements. LGED reviews the subproject proposals for technical, engineering, environmental, social development, and safeguards compliance before allocation of the financing to the ULBs.

Social Management Plans (SMP) will be prepared and implemented for subprojects with no land acquisition or involuntary displacement of people, while Resettlement Action Plans (RAP) and

Tribal Peoples Plans (TPP)) will be prepared for implementation for subprojects involving population displacement and tribal peoples' issues.

1.3. Brief Description of Subproject

Subproject No. MGSP/ ELN/2017-18/W-04.

Brief Description of Subproject

The propose subproject comprises two works as under

a) Construction of 33m RCC Bridge at Pathalkandi Channel under EllengaPourashava, Kalihati, Tangail

b)Construction of approach road by bituminous carpeting for 33m RCC Bridge at Pathalkandi Channel (Ch.0+000 to 0+103 m, 0+136 to 0+ 970 m and link 160 m)under Ellenga Pourashava, Kalihati, Tangail.

This subproject is an important construction work of Ellenaga Pourashava to be implemented in the ward numbers4 & 6. The proposed bridge will be constructed on the Pathalkandi canal for establishing better communication between two wards of Ellenga Pourashava located on two sides of the canal. The existing connectivity is a bamboo footbridge constructed and maintained by EllengaPourashava.

The required land for bridge construction belongs to the Ellenga Pourashava entirely including approach road. In the west bank of the existing canal there is 864 m long (Ch.0+136 to 0+ 970 m) earthen road and in the east side there is 103 m long (Ch.0+000 to 0+103 m) earthen road and 160 m linked road,(Ch.0+000 m to 160 m) also earthen.



Figure 1.Existing Foot Bamboo Bridge Condition

subproject connects residential and commercial areas. Beside the approach and link roads, there are several types of shops like grocery, stationary and tea stall. One Government Primary School (GPS) and beside the approach road. About 10,000 people comprising businessmen,

The existing bamboo bridge is old, damaged and it is very muchrisky to cross the canal using this bridge. In rainy season and flood time it is very difficult for women, children elderly peoples, students, traders, farmers and sick peoples to cross the canal.

Due to not having a bridge, vehicles coming from the town have to stop in east side of the canal. Rickshaws coming from the west side cannot go to the main road and must stop beside the canal. This bridge will therefore remove the difficulty of movement. This

students, children, women, teachers and services holders will benefit from the use this bridge. The bridge and approach roads will be used by all types of transports like, rickshaw-van, CNG driven baby taxi and battery operated three wheeler auto, pickup, microbus and Lorries.

During the screening work the DSM and ULB team noted all existing facts & features of proposed subproject areas including difficulties as well as scopes and opportunities. During social screening the DSM team has seen as following:

- There is no obstacles were seen in the subproject areas.
- To construction of subproject need shifting of 04 electrics poles and felling of about 30 trees (Zika, Mango, Eucalyptus, betel nut and coconut).

After construction of this bridge with approach and link roads, the wards 4 & 6 of Ellenga Pourashava will have better transport network. It will minimize difficulties in movement for vehicle & people, will save travel time and cost and reduce traffic congestion for working as a localized by-pass to national highway.



It will improve market linkage, rural-urban link, and therefore will provide opportunity to market agricultural and other produce. At the one side, the producers will get better price and consumers will get commodities of better quality at lower price. Further it will contribute to increased business opportunities, employment, income, living standard and reduced poverty. For the bridge, movement will be safer for all categories of people including

Figures. Peoples faced difficulties due to risky bridge

women, children, elderly persons and disabled. Besides economic the subproject will improve access to education, health

and administrative services. The sub project bridge approaches and link road are all located on ULB's own land there is no need for land acquisition. So it is not necessary to prepare RAP. Also there is no need for voluntary dispossession as no property is affected.

The proposed subproject and its estimated costs are given in the following table.

SI	Subprojects	Length (m)	Estimated Cost (BDT)	Remarks
a	Construction of 33 m RCC Bridge at Pathalkandi Channel under Ellenga Pourashava, Kalihati Tangail	33m	20,452,663.22	RCC bridge
b	Construction of approach road by bituminous carpeting for 33 m RCC Bridge at Pathalkandi Channel (Ch.0+000 to 0+103 m, 0+136 to 0+ 970 m and link 160 m)under Ellenga Pourashava, Kalihati,	1160 m	17,274,493.02	Approach & Link road

SI	Subprojects	Length (m)	Estimated Cost (BDT)	Remarks
	Tangail.			
	Total		37,727,156.25	

1.4 Social Management Plan

This Social Management Plan (SMP) has been prepared for the identification and managing social concerns in the process of subproject design and implementation. Initially, each subproject component included a social screening report. After reviewing the social screening report by Social Unit of DSM, this subproject proposal is submitted to the PMU office. Social team of the DSM has carried out a site visit and consulted with the Rangpur City Corporation officials, stakeholders (down to the ward level) and cross section of the beneficiary communities through interview, participation and consultation meeting. The social screening findings from the consultation method and proceedings were further validated through the social survey and consultation.

The purpose of preparing this SMP is to demonstrate an all-inclusive consultative process in selection and design of the subproject as well as to provide guidance for social development and safeguards compliance in the implementation process.

The SMP contains a description of the subproject area, social screening and impacts, consultation process adopted during identification and design, and consultation plan for implementation stage, impact mitigation measures, grievance resolution process, and implementation arrangements, and monitoring and evaluation.

Elega Municipal authority will ensure participation of the communities and grievance resolution in the process of implementation of the subprojects.

2. Description of Subproject Area

2.1. Brief Profile of Ellenga Pourashava

Ellenga Pourashava is a town at Kalihatiupazila under Tangail District. Ellenga Pourashava is situated 10 km north-east from Tangail District headquarters. The Lowhajong River flows near the west side of Ellenga Pourashava. The Pourashava was established in 2011. It is now 'C' Category Pourashava. It has 09 wards with a present area 23.254 sq. km. Total Population of Ellenga Pourashavais 42,597 (Male: 20,841 and Female: 21,756) as per census 2011 and at present total population is 50,860 (Male: 24,922 and Female: 25,938) of 2017 as per growth rate 3.00%. The number of household is 10,800 as per assessment 2012. The population density and average household size is 2188 and 4.71. Ellenga is renowned as a cultural town. It has turned into an important trade center for its excellent communication and transportation network with rest of the country including the capital city. In 1998 the Jumuna Multipurpose Bridge connected Ellenga to the national highway and since 2003 it has been added to the national railway map. It is the gateway to the 4.80 km long Bangabandhu Bridge and the middle of Bangladesh.

2.2 Historical and Cultural Heritage Sites:

Under the Ellenga Pourashava, there is some historical and Archaeological heritage sites established these are as follows:

Ellenga Resort, Ellenga Old Landlord House, Fultala Ashrtafia Jame Mosque and Bismilla Horticulture, Fultala.

At a glance Ellenga Pourashava Kalihati, Tangail

General Information		
Area	:	23.254 Sq.km
Wards	:	09 no's
Population	:	50860(Male :24922& Female :25938)
Population Growth Rate	:	3.00%
Households	:	10800
Population Density	:	2188
Main Income Sources	:	Business & Agriculture
	:	
Tax		
Holding Tax (People Property)	:	50,80,671.00
	:	
Market		
Markets	:	19
Shopping Complexes	:	02
Private Shops	:	2250
	:	
License		
Trade Licenses	:	1500
Rickshaw Licenses	:	390
	:	
Communication Infrastructure		
Road (KM)	:	
a) Bituminous	:	45.50
b) CC	:	2.00
c) RCC	:	-
d) Brick Flat Soling	:	2.00
e) Nonpaved Roads	:	67.00
Culverts	:	49 no's
Road intersections	:	04
Bridges	:	06 no's
Footpaths	:	2.87
	:	
Water Supply		
Overhead Tank s	:	Nil
Capacity	:	Nil
Deep Tube wells	:	Nil
Pipe Lines	:	Nil

House Connection	:	Nil
Street Hydrant	:	Nil
Pump Station/ house	:	Nil
Arsenic Status	:	Nil
Pumping Hour	:	Nil

Street Lighting		
Electricity Line	:	
Light Posts	:	95 no's
Tube Light Points	:	Nil
Energy Savings Light Point s	:	95
Meter or Switch Points	:	03

Vehicle		
Garbage Trucks	:	02 no's
Tractor	:	Nil
Bulldozers	:	Nil
Jeeps	:	01 no
Motorcycles	:	02
Hydraulic Beam Lifters	:	01
Road Rollers (4 ton – 6 ton)	:	02
Road Rollers (8 ton – 10 ton)	:	Nil
Excavators (8 Cft)	:	Nil
Bulldozers	:	Nil

Drainage System		
Drain	:	
a) RCC or Brick Lined Drains	:	3700 m
b) Earthen Drains	:	500 m
Canal	:	06 no's

Public Health		
Public Toilets	:	07 no's
Sanitary Latrines	:	400 no's
Dustbins	:	37 no's
Sanitary Land fill	:	Nil
EPI Centre Permanents	:	15 no's
Temporary	:	Nil
Hospitals	:	01 no
Clinics (TB)	:	01
Maternity Clinics	:	Nil
Private Clinics	:	07 no's
Diabetic Hospitals	:	01 no

UPPRP Project		
Community Centers	:	Nil
Total Urban Communities	:	Nil
Total Community Families	:	Nil

Total Community Members	:	Nil
Total Community Deepest	:	Nil
Total Sanitary Latrines	:	Nil
Total Roads with Footpaths	:	Nil
Drains	:	Nil
Improved cooking	:	Nil
UPHSDP Project		
Nagar MatriSadan	:	Nil
Nagar Health Centre	:	01
Satellite Clinics	:	05 no's
Religious Institutions		
Mosques	:	53 no's
Eidghahs	:	21 no's
Temples	:	13 no's
Graveyards	:	25 no's
Burning Ghats	:	05 no's
Church	:	Nil
Buddhist Temple	:	Nil
Orphanages	:	03 no's

Law & Order		
Police Stations	:	01
Police Fari	:	01
Judge court	:	Nil
Others	:	Nil

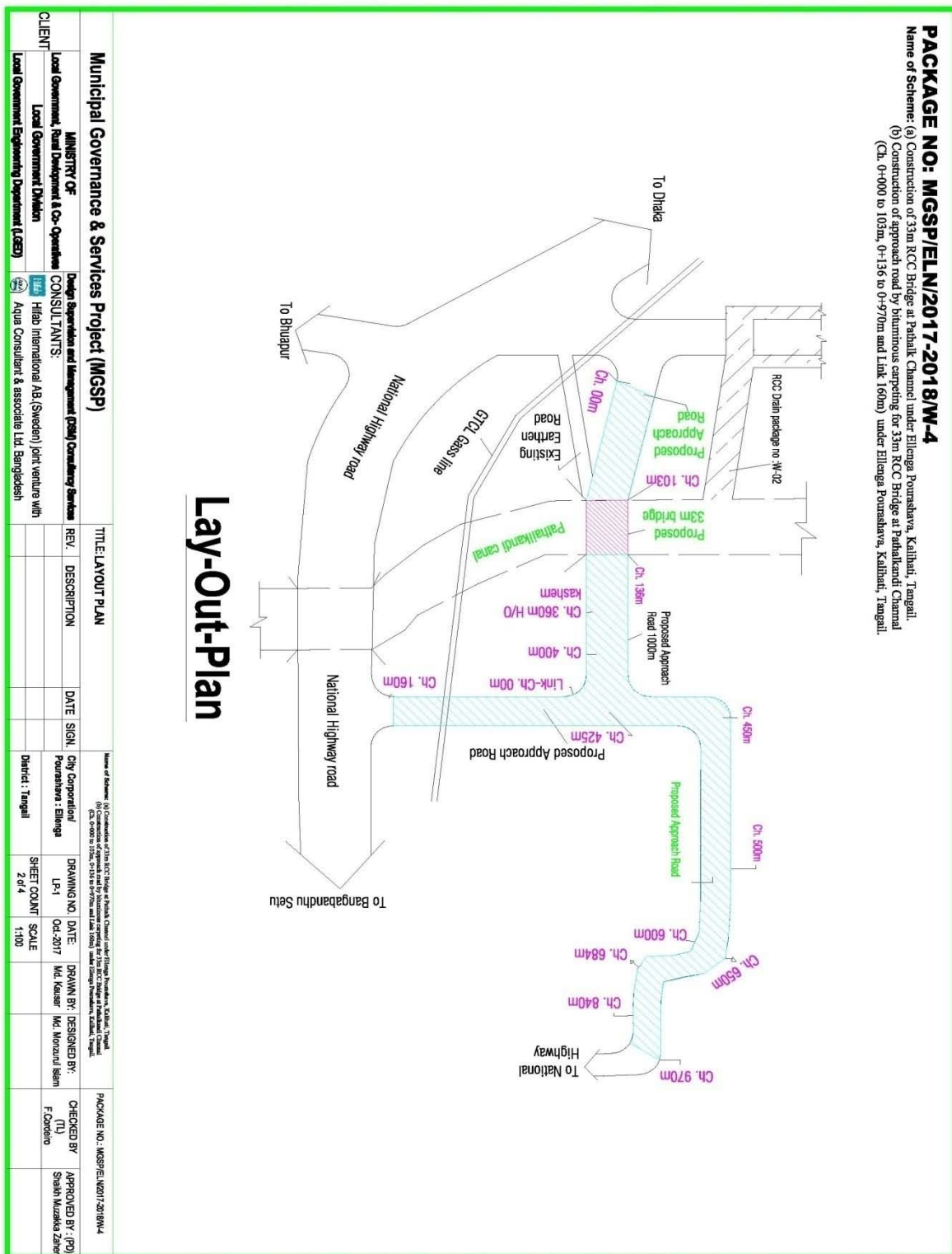
Education		
Public University	:	Nil
Private University	:	Nil
Govt Colleges	:	01
Private Colleges	:	02
Cadet College	:	Nil
Public Medical College	:	Nil
Private Medical Colleges	:	Nil
Art College	:	Nil
Law Colleges	:	Nil
Homoeopathic Medical College	:	Nil
Public Polytechnic Institute	:	01
Public Engineering Survey Institute	:	Nil
Private Polytechnic Institutes	:	Nil
Madrasha	:	06
Teachers Training College	:	Nil
Govt High Schools	:	Nil
Non-Govt High Schools	:	05 no's
Govt Primary Schools	:	15 no's

Recreation		
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<i>Park</i>	:	Nil
<i>Zoo</i>	:	Nil
<i>Cinema Halls</i>	:	01
<i>Stadium</i>	:	Nil
<i>Auditorium</i>	:	Nil
<i>Gymnasiums</i>	:	Nil
<i>Recreation Clubs</i>	:	12

Communication		
<i>Railway Station</i>	:	Nil
<i>Bus Stations</i>	:	01
<i>Ferry Ghat</i>	:	Nil
<i>T & T Office</i>	:	01
<i>GPO</i>	:	Nil
<i>Post Offices</i>	:	03

Figure: 4. Subproject layout plan (W-4) I Ellenga Pourashava.



2.2. The Subproject Location and Area Profile

Name of Subproject Components:

The sub project components noted above have two Government Primary educational institutions; mosque, Fishery foundation office and these will all be benefited by the subproject interventions comprises a 33 m RCC bridge, 937 m approach road and 160 m link road.

This subproject benefited areas are mainly commercial & residential. The bridge to be provided safe communication facilities and have a variety of commercial enterprises like grocery shops, tea stall & restaurant etc.

Various types of vehicles like battery operated three wheeler auto, CNG operated three wheeler baby taxi, rickshaw, rickshaw van etc. will cross the bridge to connect Ward 6 to town-side ward 4. Also modern vehicles like car, jeep, pick-up, bus and Lorries will be using the bridge. The pedestrians, particularly women, children, elderly people will use the bridge & approach road more comfortably and safely.

2.4. Beneficiary Communities and Affected Persons

The proposed bridge & approach road will help improving road condition and remove traffic congestion. As a result people will be able to move easily all over the year. Local transports like rickshaw, rickshaw van, battery driven three wheeler auto and CNG driven baby taxi and diesel operated tractor trailers (Bhotbhoti) will move conveniently besides covered van, cars, micro buses, pickup van and other local transports.

This part of the town is quickly growing residential cum commercial area. About 10,000 people will be benefited by the subproject infrastructure of bridge and approach road. They will use the bridge and approach road to meet the needs of job holders, students, children & women. People of two wards will (4 & 6) mainly benefit from the sub project. In addition, people of other wards of the town and the adjoining others wards will use the bridge & approach road. The proposed bridge and approach road will establishing overall town safely communication net works situation with the connection of national high ways and as well as intra town connectivity networks system.

This is very densely populated area. It is noted here that there is no presence of tribal people in the subprojects influence areas.

2.5. Gender and Vulnerability

Gender and vulnerability analysis have been considered in social impact assessment of subprojects. This has focused gender and vulnerability based on findings from specific queries during social screening and community consultation. The quantitative and qualitative analysis has brought out sex disaggregated data and issues related to discrimination by gender vulnerability, needs, constraints, and priorities as well as understanding whether there is a potential for inequitable risks, benefits and opportunities relating to gender and vulnerability. Based on the social analysis, specific interventions inclusion and participation encourages gender mainstreaming in the project cycle. Gender equity and empowerment will be ensured through encouraging participation of men and women equitably in the project

cycle. The community participation and consultation also encourages gender mainstreaming in the subproject influence areas.

After subprojects implementation, there are positive impacts for the vulnerable women; especially those who are small traders, landless, marginal farmers, etc. and can sell their household products. These subprojects will create job creation and livelihoods. In addition, it will also increase the mobility of drains users. Due to the fact, gender and vulnerability has been taken into account and ensures the gender equity in the subprojects implementation.

3. Social Impact Assessment

3.1 Benefits of the Subproject

After identification of subproject through CIP, PMU of the project with the assistance of DSM Consultants has completed social screening and socio-economic impact assessment for the bridge, approach and link road of Ellenga Pourashava. This subproject is predominantly used for inter-town and intra-town movement. It will improve traffic network and reduce and improve stability of the road at lower maintenance cost. From the point of view of social benefits it will enhance commercial/economic activities, direct/indirect employment opportunity and poverty reduction. Also it will improve access to health service, educational institution. For improved transport network, property value of adjoining area will increase substantially, rent will increase and this will eventually contribute to increased revenue earning for the ULB. In the two wards of the Ellenga Pourashava, the subproject interventions will benefit about 10,000 people which are about 20% of the ULB population as of 2011.

3.2 Social Screening and Safeguard Compliance Issue

After selection of the subprojects from the CIP list, Ellenga Pourashava with the assistance of DSM Consultants has completed a social screening for the subprojects using an inclusive and participatory approach.

Ellenga Pourashava has ensured the implementation of subprojects following socially inclusive and gender friendly. According to engineering design, implementation of the subprojects will be carried out within the existing right-of-way.

Results from the social screening are given below.

- No additional public or private lands will be required for the subprojects outside the existing right of way.
- The sub project will not affect access to common property resources for any community. It will also have no negative impact on cultural property of any kind.
- Preparation of RAP will not be required for implementing the subproject as no LA is involved.
- Agricultural or industrial productivity will be enhanced by the proposed subproject.
- The outcomes of social screening also confirmed that local people across the subprojects are positive about its implementation.
- There is no obstacles has been seen at project areas but smoothly implementation this subproject needs cutting of 30 trees and shifting of 4 electric poles..

4. Consultation and Community Participation

4.1. Stakeholder Analysis

According to BBS 2011, (Collected from Municipal Authority) the total population of Ellenga was 50860 among which 249225 are male and 25938 are female. People belonging to all income groups living in higher, higher-middle, lower-middle, and lower income groups in the Pourashava area will be benefited from the subproject interventions. Both males and females living in the Ellenga Pourashava and people of adjoining rural areas will also be benefited by the construction of bridge and 937 m long approach road and 160 link road provisions.

As a part of the overall assessment, the subprojects identified the key stakeholders of the proposed subprojects areas and assessed the power relationships as well as influence and interests of stakeholders involved in the development work of the subprojects. Key stakeholders for subprojects under MGSP were identified in consultation with the Mayor and officials of Ellenga Pourashva, civil society and local people of towns, representatives of business associations and local contractors etc. Suggestions and direction of all local stakeholders were noted and reflected in the SMP while designing subproject interventions.

4.2. Consultation and Participation plan for subproject

Participatory public consultations were conducted in the subprojects area. Key Informant



Figure 2 : Community consultation at Pathalkandi

Interviews (KII) and Group Discussion (GD) were conducted involving the participants of Ellenga Pourashava Mayor, Councilors, and Pourashava officials, representatives of local communities and civil society members as well as representative of local LGED office and consultant team

A walk-through informal group consultation with the local communities was also held. Through this participation and consultation meeting, the local communities were informed about subprojects implementation and benefits. Suggestion and recommendations made by the participants are incorporated in the SMP accordingly.

4.3. Consultation Outcomes – Issues, Concerns, and Recommendations

The participants raised their issues related to subprojects improvement which mainly RCC bridge. Feedback, suggestions and recommendations by the participants are listed below.

- Construction works should be scheduled properly and the quality of construction work should be maintained strictly.
- Local women labor will be engage on priority basis together with ensuring women's participation in project design, implementation and monitoring stages.
- Social safeguard compliance issues will be enhanced and ensured through inclusive participation of subprojects beneficiaries in subprojects implementation.
- Any problem arising out of labor influx will be carefully seen by the ULB and mitigation measures taken.

5. Social Management for site Selection and design.

5.1. Subproject Selection Process

MGSP in coordination with concerned Ellenga Pourashava elected functionaries, local administration, relevant stakeholders, community members, bridge users and civil society members followed the participatory approach in different stages of subprojects selection following inclusion, consultation and participation methods. Female Ward Councilor participated actively in the selection process. Moreover, the representatives of TLCC and WC from the Ellenga Pourashava have contributed in the subprojects selection process with an analysis of the inclusiveness of the selection process.

At the time of selecting these subprojects, Social Screening and Group Discussion (GD) with Ellenga Pourashava urban communities, local administration, traders, buyers, and sellers were conducted by the Ellenga Pourashava officials and Consultants from DSM. Views and opinions of these consultations were to explain the subproject objectives and sought feedback from the participants to maximize the social and economic benefits as well as to minimize the adverse impacts of the subprojects.

5.2. Subproject Design Process

After final selection of the subprojects, the DSM Consultant designed the proposed subproject (bridge) under Ellenga Pourashava. These subproject components will be constructed using Ellenga Pourashava own land. Despite the inclusion and participatory consultations, if any person has a grievance relating to the implementation of the subprojects, the Pourashava will mitigate it.

The Consultant interviewed stakeholders and beneficiaries of the subprojects areas before the design. The Consultant and Pourashava officials conducted a number of consultation meetings in the subprojects areas discussing the implementation procedures and mitigation measures, if any, required to be taken in implementing the proposed subproject.

5.3 Grievance Redress Mechanism (GRM):

GRM had already operationalized for Ellenga Pourashava. Accordingly, this Pourashava has formed Grievance Redress Committee (GRC) to handle any grievance raised due to implementation of the subprojects. The committee is headed by the Mayor of the Ellenga Pourashava and consists of total 7 members. The committee will resolve subproject-related issues, answer to queries and address complaints and grievances about any irregularities in application of the guidelines adopted for assessment and mitigation of social and environmental impacts. Based on consensus, the procedure will help to resolve issues/conflicts amicably and quickly without resorting to any expensive, time-consuming legal actions. It will ensure proper presentation of complaints and grievances, as well as impartial hearings and transparent decisions.

Ellenga Pourashava has already appointed focal Point of GRC who is at the level of Assistant Engineer. But he will also act Social Development Officer for the MGSP subprojects implementation in their Pourashava. The Mayor of the Ellenga Pourashava is the Chairman of the GRC. This GRC at the Pourashava level is responsible to disclose the subprojects implementation before civil works starts.

Structure of the GRC Committee:

Person	Status
ULB Mayor	Convener
Representative of Local Administration	Member
Teacher from a Local Educational Institution	Member
Representative of a Local NGO	Member
Representative of Civil Society	Member
Female Ward Councilor	Member
Head of Engineering Section of ULB	Member Secretary

5.4 Implementation Arrangement: DSM has initially completed social screening for the subprojects and there is no problem for implementing the subprojects regarding social safeguard issues. Accordingly, PMU, MGSP has issued the award letter against the subproject following all required procurement procedures Ellenga Pourashava has the responsibility to implement the subprojects timely in close coordination with the PMU of the project and also with the technical assistance of DSM. Engineering section of Pourashava and local representative of Consultancy team will ensure the quality construction work of the subprojects. In addition, Consultant team from Head Quarters DSM will ensure close monitoring of the implementation of the subproject components. In the implementation process, Social Safeguard Team will ensure monitoring of social safeguard management before civil works start. During construction, the communities will not be affected and the project will minimize the loss of communities. In addition, before civil works start, the Ellenga Pourashava will make the video filmed and also social screening.

6. Conclusions on Social Management Plan (SMP)

The subproject will improve communication networks in two wards of the Ellenga Pourashava providing more convenient and stable road connectivity in the Ellenga Pourashava area and with the adjoining rural areas. About 10,000 people of two wards and many others of adjoining areas will be benefited. Implementation of the subproject will need cutting 30- tree and 4 electric poles relocated.

Annex-01 .Social Screening Report



Annex -1

Local Government Engineering Department (LGED)
Municipal Governance and Services Project (MGSP)
Agaragaon, Dhaka-1207
SOCIAL SAFEGUARDS SCREENING OF SUBPROJECTS
REVIEW and EVALUATION
REPORT ON SOCIAL SCREENING

PACKAGE NO: MGSP/ELN/2017-18/W-4

Name of Subproject Component

- a) Construction of 33 m RCC Bridge at Pathalkandi Channel under Ellenga Pourashava, Kalihati, Tangail
- b) Construction of approach road by bituminous carpeting for 33 m RCC Bridge at Pathalkandi Channel (Ch.0+000 to 0+103 m, 0+136 to 0+ 970 m and link 160 m)under Ellenga Pourashava, Kalihati, Tangail.



Design, Supervision, and Management (DSM) Consultancy Service

Joint Venture of *Hifab International AB, Sweden*



AQUA Consultant & Associates Ltd., Bangladesh



SEPTEMBER -2017
ELLENGA POURASHAVA.
KALIHATI, TANGAIL

FORM 1: SOCIAL SAFEGUARDS SCREENING OF SUBPROJECTS REVIEW and EVALUATION

Description of subproject:

The subproject comprises two works as under:

a) Construction of 33 m RCC Bridge at Pathalkandi Channel under Ellenga Pourashava, Kalihati, Tangail

b) Construction of approach road by bituminous carpeting for 33 m RCC Bridge at Pathalkandi Channel (Ch.0+000 to 0+103 m, 0+136 to 0+ 970 m and Link 160 m) under Ellenga Pourashava, Kalihati, Tangail.

This subproject is an important construction work of Ellenga Pourashava to be implemented in the ward numbers 4 & 6. The proposed bridge will be constructed on the Pathalkandi canal for establishing better communication between two wards of Ellenga Pourashava located on two sides of the canal. The existing connectivity is a bamboo footbridge constructed and maintained by Ellenga Pourashava.

The required land for bridge construction belongs to the Ellenga Pourashava entirely including approach road. In the west bank of the existing canal there is 864 m long (Ch.0+136 to 0+ 970 m) earthen road and in the east side there is 103 m long (Ch.0+000 to 0+103 m) earthen road and 160 m linked road, (Ch.0+000 m to 160 m) also earthen.

The existing bamboo bridge is old, damaged and it is very risky to cross the canal using this bridge. In rainy season and flood time it is very difficult for women, children elderly peoples, students, traders, farmers and sick peoples to cross the canal.

Due to not having a bridge, vehicles coming from the town have to stop in east side of the canal. Rickshaws coming from the west side cannot go to the main road and must stop beside the canal. This bridge will therefore remove the difficulty of movement.

This subproject connects residential and commercial areas. Beside the approach and link roads, there are several types of shops like grocery, stationary and tea stall. One Government Primary



Figure 1 Present condition of Pathalkandki channel

School (GPS) and beside the approach road. About 10,000 people comprising businessmen, students, children, women, teachers and services holders will benefit from the use this bridge. The bridge and approach roads will be used by all types of transports like, rickshaw-van, CNG driven baby taxi and battery operated three wheeler auto, pickup, microbus and Lorries.

During the screening work the DSM and ULB team noted all existing facts & features of proposed subproject areas including difficulties as well as scopes and opportunities. During social screening the DSM team has seen as following:

- There is no obstacles were seen in the subproject areas.

- To construction of subproject need shifting of 04 electric poles and felling of about 30 trees (Zika, Mango, Eucalyptus, betel nut and coconut).

After construction of this bridge with approach and link roads, the wards 4 & 6 of Ellenga Pourashava will have better transport network. It will minimize difficulties in movement for vehicle & people, will save travel time and cost and reduce traffic congestion for working as a localized by-pass to national highway. It will improve market linkage, rural-urban link, and therefore will provide opportunity to market agricultural and other produce. At the one side, the producers will get better



Figure 2 Existing approach road condition

price and consumers will get commodities of better quality at lower price. Further it will contribute to increased business opportunities, employment, income, living standard and reduced poverty. For the bridge, movement will be safer for all categories of people including women, children, elderly persons and disabled. Besides economic the subproject will improve access to education, health and administrative services.

The sub project bridge and linked approaches are all located on ULB's own land there is no need for land acquisition. So it is not necessary to prepare RAP. Also

there is no need for voluntary dispossession as no property is affected.

2. INTRODUCTION:

This screening will enable best effective assessment of social issues concerning the subproject component and it has been completed as accurately as possible based on factual information. This screening also enabled the best effective assessment of social matters in the level and it has been completed as accurately as possible based on factual evidences.

3. OBJECTIVES AND GOALS:

The primary objective of this Social Screening is to check that relevant social issues are properly identified are remedial measures indicated so that sub projects involving unmanageable social risks are screened ,hence the PMU can decided which ones to include or exclude which ones should have further documents prepared like SIMP

4. STRATEGY OF SOCIAL SCREENING:

The Social Screening is based on a collection of primary data and information while conducting field survey for the subproject. A well-structured questionnaire was already designed in the Social Management Framework and given to the field officials for obtaining necessary primary data relating to social issues in connection with losses of land and any kind of assets for the subproject implementation. Further information was obtained through field visits and interviews with relevant stakeholders to capture collective opinions and feedback by using "Group Discussion (GD)"and "Key Informant Interviews (KII)".

5. TEAM COMPOSITION:

The Social Screening analysis based on the received documents on subproject related social safeguards aspects from the ULB was conducted by following members:

- a) Md. A .B .Siddique. SMO, Rangpur Zone ,MGSP, ,LGED
- b) Md. Abdul Quddus Khan, Senior Municipal Engineer, Rangpur Zone, MGSP, LGED
- c) Md. Jahurul Hoque Assistant Engineer , Ellenga Pourashava
- d) Md. Abdul Motalab , Assistant Municipal Engineer, DSM, Ellenga Pourashava, MGSP, LGED

6. METHODOLOGY OF SOCIAL SCREENING REPORT ON SUBPROJECT:

Subproject screening was carried out as per Social Management Framework of MGSP with the use of a prescribed social screening format. The concerned Social Management Officer and Senior Municipal Engineer, Assistant Municipal Engineer and Sub-Assistant Engineer conducted the social screening and the concerned Councilors of the Pourashava participated in it. The screening was based on participatory consultation with the local people using Group Discussion (GD) and Hot Spot (on the spot) consultation with different categories of local people who are aware of local situation and needs of the subproject area.

Focus of the screening includes social safeguard compliance issues such as loses of land, displacement of population, stakeholder participation, income, gender, vulnerability and presence of tribal people etc. The field visit reports on social screening were prepared by the Social Management Officer and the Social Management Specialist has reviewed the reports. The DSM ensured that nobody in the subproject area will be adversely affected or deprived due to the implementation of the subproject, and the PAPs (if any) are not forcibly evicted. However, it was found that there is no obstacles were seen for smoothly construction of bridge and approaches road.

Ellenga Pourashava and Consultants jointly ensured the consultation with the communities, infrastructure users, elected public representatives, local administration, teachers, Businessmen, NGOs, social workers and civil society members.

7. Subproject Implementation:

The sub project will take necessary steps regarding implementation of the subproject. Based on design, the ULB and the DSM will identify PAPs, make their inventory and will produce documentation of agreements on negotiated settlement using legal stamps, where necessary. It is confirmed that RAP will not be required as land acquisition and physical relocation are not involved.

8. KEY FINDINGS

8.1 LAND REQUIREMENTS:

Based on the Social Screening, from the field, it is confirmed that there is no need for land acquisition and preparing RAP. The concerned bridge and approaches road site belongs to the Ellenga Pourashava. There is no need to take other any public agencies (such as BWDB, R&H, Railway etc) for the sub project and there is no leaseholder of ULB or of any GOB agencies affected by it.

8.2 HOMESTEAD AND OTHER NON-LAND ASSETS:

It is noticed that no land of the sub project component belongs to private citizens. So no need any land acquisition for subproject implementation because required land belongs to the Ellenga Pourashava.

8.3 INCOME LOSS/ BUSINESS LOSS:

Social Screening results demonstrate that the subproject component does not affect any business or income source.

The proposed subproject is not expected to affect any community group's access to any resources that are used for livelihood purpose. None of the cultural heritages are going to be affected.

8.4 INDIGENOUS PEOPLE (IP), ADHIBASHI, and MARGINALIZED POPULATION:

There is no Indigenous, Adhibashi and marginalized population that will be affected during subproject implementation. The survey team did not find any Indigenous, Adhibashi or marginalized population living besides the sub project.

9. CONCLUSIONS AND RECOMMENDATIONS:

The sub project will have tremendous positive impact on the construction of bridge, approach road and link in the Pathalkandi area directly benefiting about 10,000 people. The proposed bridge will be connected to the main town communication net works of Ellenga Pourashava and will reduce dwellers travels time of implementation of the proposed subproject is highly recommended.

- In this proposed subproject areas there are no obstacles were seen but
- To construction of subproject needs shifting of 04 electric poles and also felling about 30 trees c (Zika Mango, Eucalyptus betel nut and coconut) will be needed. .

This report has been prepared following filled up questionnaire and checklist which follow

FORM 1: SOCIAL SAFE GUARDS SCREENING OF SUBPROJECTS

REVIEW and EVALUATION

Name of Subproject:

a) Construction of 33 m RCC Bridge at Pathalkandi Channel under Ellenga Pourashava, Kalihati, Tangail

b) Construction of approach road by bituminous carpeting for 33 m RCC Bridge at Pathalkandi Channel (Ch.0+000 to 0+103 m, 0+136 to 0+ 970 m and link 160 m)under Ellenga Pourashava, Kalihati, Tangail.

Name of District : Tangail Upazila: Kalihati Name of ULB: Ellenga

I. Checklist for Exclusion

1) Subproject requires land acquisition and/or population displacement that need to be compensated for or resettled	<input type="checkbox"/> Yes	<input checked="" type="checkbox"/> No
2) Subproject affects mosques, temples, graveyards and cremation grounds, and other places/objects of religious, cultural and historical significance.	<input type="checkbox"/> Yes	<input checked="" type="checkbox"/> No
3) Subproject threatens cultural tradition and ways of life of tribal peoples; severely restrict their access to common property resources and livelihood activities.	<input type="checkbox"/> Yes	<input checked="" type="checkbox"/> No
4) Communities have objections on subproject interventions on social and environmental issues those cannot be resolved through design alternatives.	<input type="checkbox"/> Yes	<input checked="" type="checkbox"/> No
II. Resettlement Impacts		
In respect of the social impacts and community concerns, is there a need to, Undertake an in-depth social impact assessment study?	<input type="checkbox"/> Yes	<input checked="" type="checkbox"/> No
Prepare a Resettlement Action Plan?	<input type="checkbox"/> Yes	<input checked="" type="checkbox"/> No
III. Impacts on Tribal Peoples		
In respect of the social impacts on tribal peoples and their concerns, is there a need to, Undertake an in-depth impact assessment study?	<input type="checkbox"/> Yes	<input checked="" type="checkbox"/> No

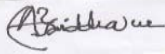
Prepare a Tribal Peoples Plan?	<input type="checkbox"/> Yes	<input checked="" type="checkbox"/> No
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On behalf of the DSM Consultant, the attached filled out format has been reviewed and evaluated by:

Decision on selection: recommended for selection recommended for exclusion

Prepared by : Md .A .B. Siddique. **Designation:** Social Management Officer, MGSP, and LGED

Signature:



Date: 28/09/2017

Reviewed by : Dr. M. Maniruzzaman. Designation: Social Management Specialist, MGSP, LGED

Signature:



Date: 28/09/2017

FORM 1: SCREENING FORM FOR SOCIAL SAFEGUARDS ISSUES

A. Identification :

1. Name of ULB: Ellenga Upazila: Kalihati. District: Tangail

2. Ward/Mahalla: Ward no. 4 & 6.

Name of subproject / spot screened:

Project component: Construction of RCC Bridge and approach road

3. Name of subproject:

a) Construction of 33 m RCC Bridge at Pathalkandi Channel under Ellenga Pourashava, Kalihati, Tangail

b) Construction of approach road by bituminous carpeting for 33m RCC Bridge at Pathalkandi Channel (Ch.0+000 to 0+103 m, 0+136 to 0+ 970 m and link 160 m) under Ellenga Pourashava, Kalihati, Tangail.

4. Brief description of the physical works

The subproject is located at ward no. 4 & 6 of Ellenga Pourashava. Under this sub project a 33 m long bridge construction and 1160 m long approach and link road (Ch.0+000 to 0+103 m, 0+136 to 0+ 970 m and link 160 m (Ch 0 to 160 m). These will built up a better communication networks between two wards (4 & 6) and for safe movement for 10,000 users. This subproject is one the most important constructions of the Ellenga Pourashva. Both banks of bridge are commercial (east side) and residential area (west side) including link. By the sides of approaches road have one Government primary school (GPS), two Mosques, Fishery foundation office and various types of shops are located near the subproject area. The subproject location is residential and commercial area. So recommend for implementation of subproject immediately.

5. Screening Date(s):28.09 .2017.

B. Participation in Screening:

6. Names of Consultants' representatives who screened the subproject:

1. Md. A. B .Siddique. SMO, Rangpur Zone, MGSP, LGED
- 2 . Md. Abdul Quddus Khan, Senior Municipal Engineer, Rangpur Zone, MGSP, LGED.
3. Md, Abdul Motalab Assistant Municipal Engineer, DSM, Rangpur City Corporation, MGSP, LGED

7. From ULB

Names of Ellenga Pourashava officials participated in screening:

1. Md. Jahuru I Hoque Assistant Engineer, Ellenga Pourashava
2. Md. Abdul Motalab Assistant Municipal Engineer, Ellenga Pourashava
3. Md. Abdul Aziz Councilor ward no :6 Ellenga Pourashava
4. Md. Sobder Ali ,Councilor Ward no: 4 Ellenga Pourashava
5. Shikha Rani Councilor reserved female ward no's, 4,5 & 6 Ellenga Pourashava

8. **WLCC members, NGOs, community groups/CBOs participated in screening: Yes, copy enclosed for information Would-be affected persons participated in screening: N/A**

9. **Would be affected persons participated in the screening: YES**

C. Land Requirements & Ownership: No.

10. Will there be a need for additional lands to carry out the intended works under this contract? yes No

11. If 'Yes', what will the additional lands be used for? (Indicate all that apply): N/A

road widening curve correction construction/expansion of physical structure

strengthening narrow eroding road section between high and low lands others (Mention):

12. If 'Yes', the required lands presently belong to (Indicate all that apply): N/A

ULB Government Land

Private Citizens Others (Mention):

D. Current Land Use & Potential Impacts

13. If the required lands belong to Private Citizens, they are currently used for (Indicate all that apply):N/A

Agriculture Number of households using the lands:

Residential purposes Number of households using them:

Commercial purposes Number of persons using them: Shops:

Other Uses (Mention): few Users:

Land does not belong to private citizen.

14. If the required lands belong to ULB and/or other Government agencies, they are currently used for (Indicate all that apply): N/A

Agriculture Number of persons/households using the lands:

Residential purposes Number of households living on them:

Commercial purposes Number of persons using them: No. of shops:

Other Uses (Mention):

15. How many of the present users have lease agreements with any government agencies? None

16. Number of private homesteads that would be affected on private lands: **NONE**

Entirely requiring relocation

Partially but can still live on present homestead: **NO**

17. Number of business premises/buildings that would be affected on private lands: None.

Entirely and will require relocation: # Of businesses housed in them:

Partially, but can still use the premises: # of businesses housed in them:

18. Residential households will be affected on ULB's own and public lands: None

Entirely affected and will require relocation: No. of these structures:

No. of structures built with brick, RCC, & other expensive and durable materials:

No. of structures built with inexpensive salvageable materials (bamboo, GI sheets, etc.):

Partially affected, but can still live on the present homestead: No of these structures

No. of structures built with brick, RCC, & other expensive and durable materials:

No. of structures built with inexpensive salvageable materials (bamboo, GI sheets, etc.):

19. No. of business premises that would be affected on ULB's own & other public lands: None

Entirely affected and will require relocation: No. of these structures:

No. of businesses housed in these structures:

No. of persons presently employed in the above businesses.

No. of these structures built with brick, RCC, & other durable materials:

No. of structure built with inexpensive salvageable materials (bamboo, GI sheets, etc.):

Partially affected, but can still stay in the present no. of these structures:

No. of businesses housed in these structures:

No. of persons presently employed in these businesses:

No. of these structures built with brick, RCC, & other durable materials:

No. of structure built with inexpensive salvageable materials (bamboo, GI sheets, etc):

20. None of businesses/ trading activities that would be displaced from make-shift structures on the road and other areas/spots: None

21. Do the proposed subproject works affect any community groups' access to any resources that are used for livelihood purposes? [] Yes [✓] No

22. If 'Yes', description of the resources: N/A

23. Do the proposed works affect community facilities like school, cemetery, mosque, temple, or others that are of religious, cultural and historical significance? [] Yes [✓] No.

24. If 'Yes', description of the facilities: N/A.

25. Describe any other impacts that have not been covered in this questionnaire? No.

26. Describe alternatives, if any, to avoid or minimize use of additional lands: N/A

E. ADDITIONAL INFORMATION ON TRIBAL PEOPLE

(This section must be filled in if subprojects are located in areas that are also inhabited by tribal peoples)

27. Names of tribal community members and organizations who participated in screening: N/A

28. Have the tribal community and the would-be affected TPs been made aware of the potential positive and negative impacts and consulted for their feedback and inputs?

[] Yes [✓] No

Has there been a broad-based community consensus on the proposed works?

[] Yes [✓] No

29. Total number of would-be affected tribal households: N/A.

30. The would-be affected tribal households have the following forms of rights to the required lands: N/A

- Legal: No. of households:
 Customary: No. of households:
 Lease agreements with any GOB agencies: No. of households:
 Others (Mention): No. of households:

31. Does the subproject affect any objects that are of religious and cultural significance to the IPs?

Yes No

G.32. If 'Yes', description of the objects: N/A.

33. The following are the three main economic activities of the would-be affected tribal households:

a. N/A, b. N/A, c. N/A

34. Social concerns expressed by tribal communities/organizations about the works proposed under the subproject: N/A

35. The tribal community and organizations perceive the social outcomes of the subproject: N/A

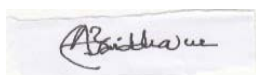
On behalf of the DSM Consultant, the attached filled out format has been reviewed and evaluated by:

Decision on selection recommended for selection recommended for exclusion

Prepared by:

Md. A. B. Siddique Designation: Social Management Officer, MGSP, LGED

Signature:



Date: 28/09 /2017

Reviewed by:

Dr.M. Maniruzzaman Designation: Social Management Specialist, MGSP, LGED

Signature:



Date: 28/09 /2017

Participation and Consultation meeting for sub projects:

A Participation and consultation meeting was held in the subproject site to carry out the feasibility of the specific subproject. Consultations with communities and other stakeholders have been used



as a two-way communication to provide information about the subproject and obtain feedbacks from the communities on subproject approach, design and implementation. Consultations were carried out through open meetings and key informant interviews (KII). From the meeting, this specific subproject is very important for safely crossing the channel over the year. The consultation process was carried out with beneficiaries of residential houses, Masjid area and shopkeeper's traders of daily market and devotes of the area.

Figure 3 Community consultation at Pathail Canal.

Local participant's attendance sheet is attached in below. (Two pages)



Social Assessment and SMF
Municipal Governance and Services Project (MGSP)

Name of Subproject: Construction of 30 m RCC Bridge at Pathalk and Channel at Ellenga (CIP-2)

Package : MGSP/ELN/2017-18 W-4.

Name of ULB : Elenga

Upazila: Kalahati

District : Tangail

Date : 28/09/2017

Attendance of Local Participants in the Screening Exercise

Local Stakeholders, community members and WLCC/CBO

SL#	Name	Gender	Social Status	Contact Number	Signature/LTI
০	শ্রী. নাহরুল	পুরুষ	মিস্ত্রিক	০১৭৩৫৬৬১৩৯	নাহরুল
২	শ্রী. রাজিব	পুরুষ	ছাত্র	০১৬৭১৬১৬৪২	রাজিব
৬	শ্রী. মতিমা	পুরুষ	প্রবাসী	০১৭৫৭৫৫৭৭২	মতিমা
৪	শ্রী. মনির	পুরুষ	ব্যক্তি	০১৭৯১০৩৪২৯	মনির
৫	শ্রী. মনির	পুরুষ	ছাত্র	০১৬৪৭০৩৪২৯	মনির
৬	শ্রী. মনির	পুরুষ	ছাত্র	০১৭৭৭৩৩৪০	মনির
৭	শ্রী. মনির	পুরুষ	ছাত্র	০১৭১৪৯০৩৭০৭	মনির
৮	শ্রী. মনির	পুরুষ	ছাত্র	০১৭১৫৩৫৪৪২১	মনির
৯	শ্রী. মনির	পুরুষ	ছাত্র	০১৭২৭ ৩৫৫৭৭৭	মনির
১০	শ্রী. আব্দুল হামিদ	পুরুষ	ব্যবসায়ী	০১৬১১৫৫৪৫৪	আব্দুল হামিদ
১১	শ্রী. মনির	পুরুষ	ছাত্র	০১৭১৬৫৫৬৫৭১	মনির
১২	শ্রী. মনির	পুরুষ	ছাত্র	০১৭১১-০১৭৬৬৭	মনির
১৬	শ্রী. মনির	নারী	ছাত্র	-	মনির
১৪	শ্রী. মনির	নারী	ছাত্র	-	মনির

শ্রী. মনির
মোঃ আব্দুল আজিজ
কমিউনিটির
এঙ্গেস পৌরসভা
কালিহাতি, টাঙ্গাইল।

শ্রী. মনির
মোঃ আব্দুল আজিজ
কমিউনিটির
এঙ্গেস পৌরসভা
কালিহাতি, টাঙ্গাইল।

শ্রী. মনির
Md. Jahurul Hoque
Asst. Municipal Engineer
Elenga Pourashava
Kalahati, Tangail

শ্রী. মনির
মোঃ শাহী খান
মেয়র
এঙ্গেস পৌরসভা
কালিহাতি, টাঙ্গাইল।

শ্রী. মনির
Md. Abdul Motaleb
Asst. Municipal Engineer
Elenga Pourashava
Tangail



**Social Assessment and SMF
Municipal Governance and Services Project (MGSP)**

Name of Subproject: Construction of 30 m RCC Bridge at Pathalk and Channel at Ellenga (CIP-2)

Package : MGSP/ELN/2017-18 W-4.

Name of ULB : Elenga

Upazila: Kalahati


District : Tangail


Date : 28/9/2017


Attendance of Local Participants in the Screening Exercise


Local Stakeholders, community members and WLCC/CBO

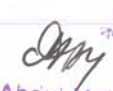
SL#	Name	Gender	Social Status	Contact Number	Signature/LTI
১৫	জামুদা	নারী	গৃহিণী	-	[Signature]
১৬	বনিয়া	নারী	গৃহিণী	-	[Signature]
১৭	চায়রা	নারী	গৃহিণী	-	[Signature]
১৮	খানাপাড়া	নারী	গৃহিণী	০১৭১৫৬২৫২৪১	[Signature]
১৯	মোহাম্মদ	নারী	গৃহিণী	০১৭২৭২৪২৫১৮	[Signature]
২০	মোহাম্মদ লেইলা	নারী	গৃহিণী	০১৭১২৬৭৩৭১	[Signature]
২১	জুয়েল মোহাম্মদ	নারী	গৃহিণী	-	[Signature]
২২	মো: হাতিয়া	নারী	গৃহিণী	০১৭১৫৫৭১৬৩০	[Signature]
২৩	ফুল মোহাম্মদ	নারী	গৃহিণী	০১৭১৩১৭৫৭২	[Signature]
২৪	করিম	পুরুষ	গৃহিণী	০১৭৭০২৫৫২৭১	[Signature]
২৫	মো: মাহমুদ	পুরুষ	গৃহিণী	০১৪৪৩৭০৫৭	[Signature]
২৬	মোহাম্মদ	পুরুষ	গৃহিণী	-	[Signature]
২৭	মোহাম্মদ	নারী	গৃহিণী	-	[Signature]
২৮	মো: মাহমুদ	পুরুষ	গৃহিণী	০১৭১৪-৪৭০৫৭০	[Signature]


 মোঃ আব্দুল আজিজ
 কাউন্সিলার
 এলেন্ডা পৌরসভা
 কালিহাতি, টাঙ্গাইল।


 মোঃ হোসেন আলী
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 Md. Jahurul Hoque
 Assistant Engineer
 Ellenga Pourashava
 Kalihati, Tangail.


 মোঃ শাহিদ খান
 মেয়র
 এলেন্ডা পৌরসভা
 কালিহাতি, টাঙ্গাইল।


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